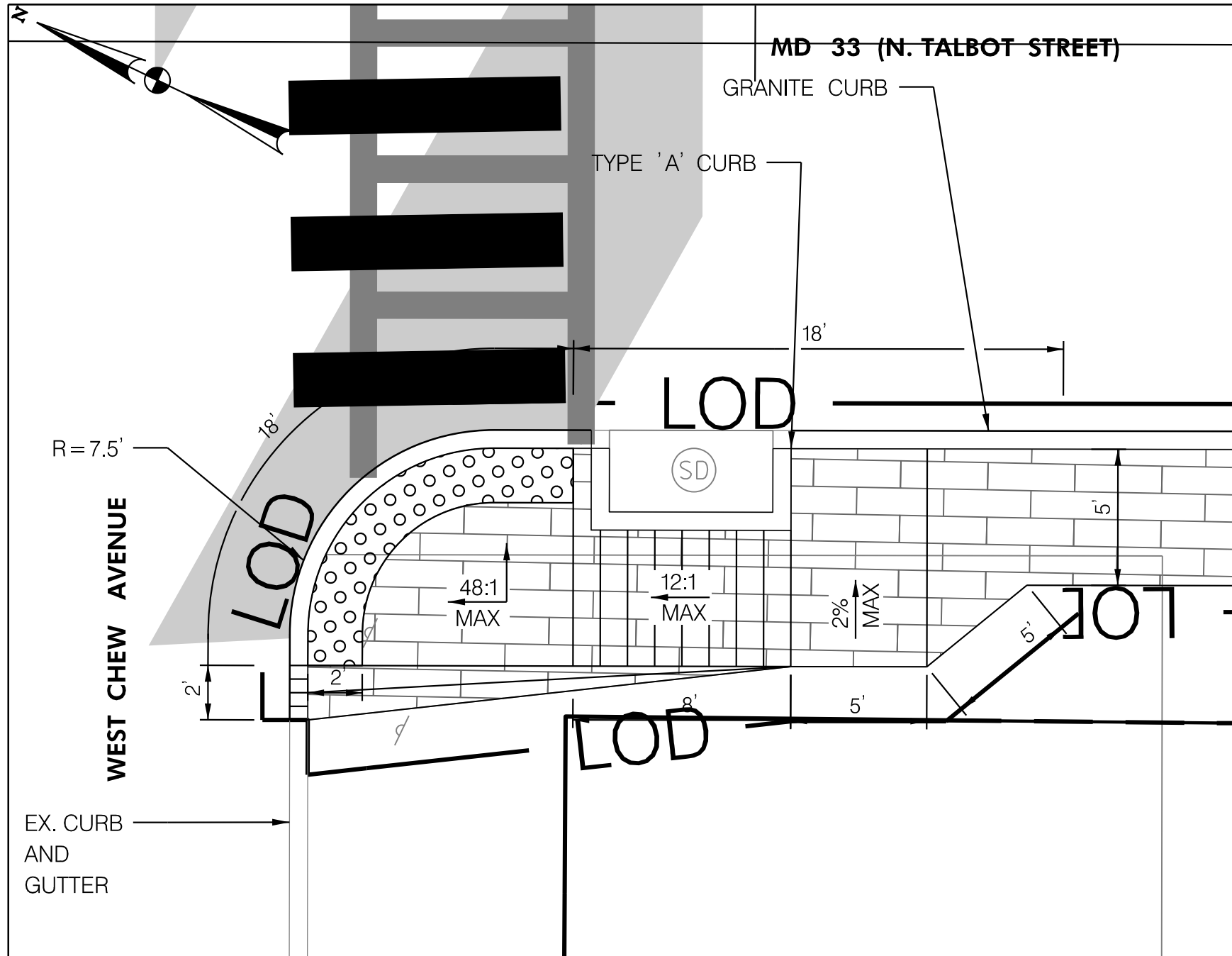
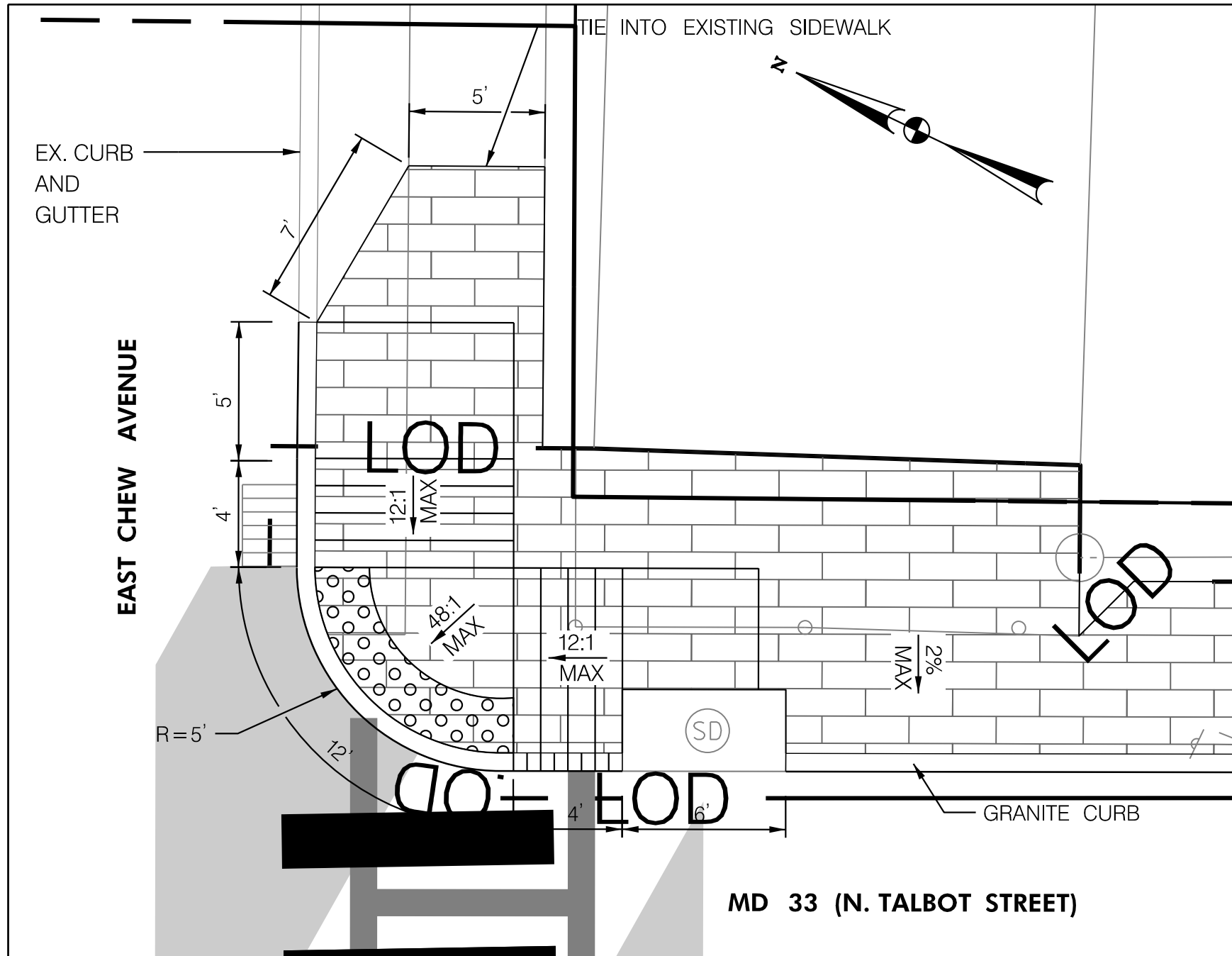


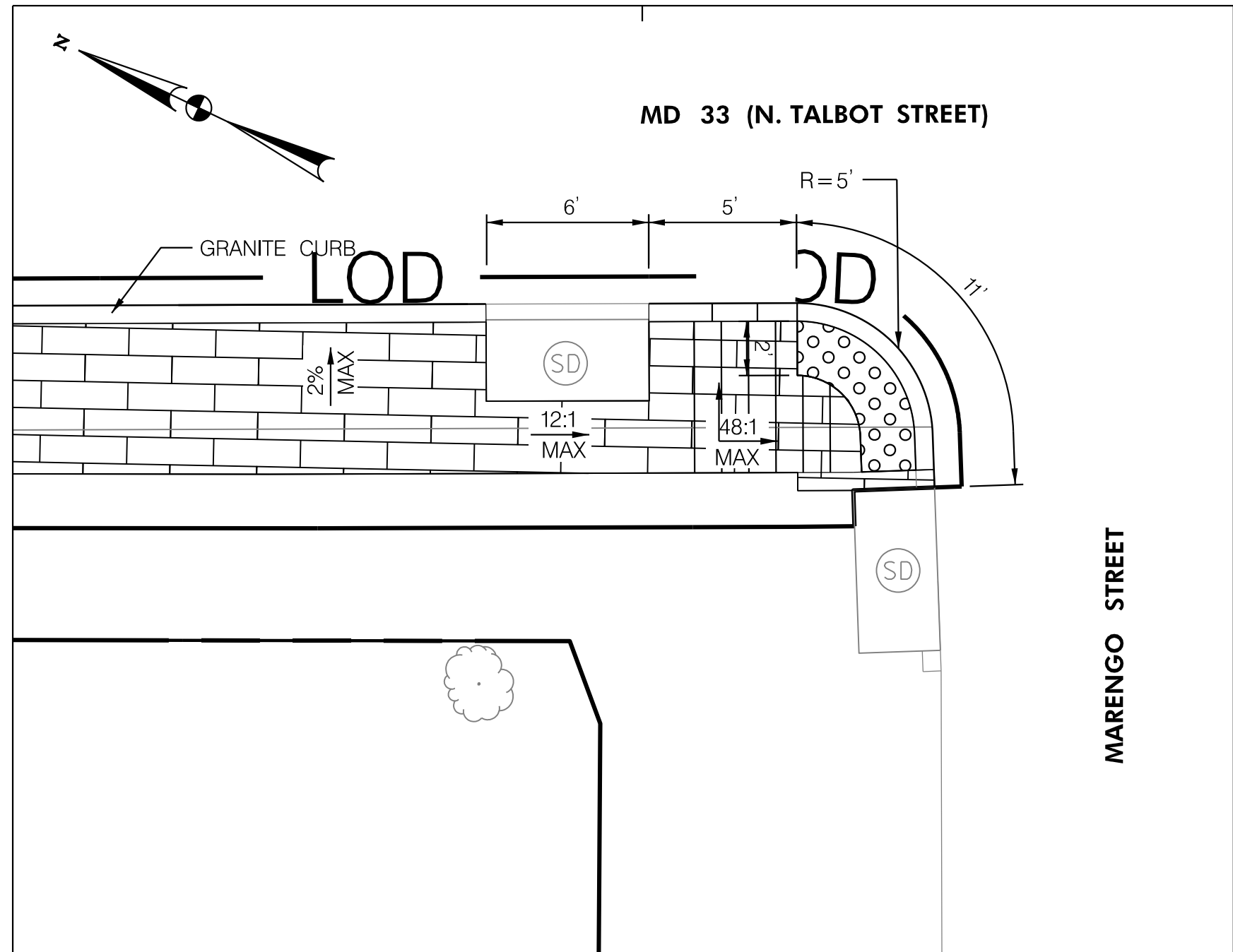
STA 57+00 RT. SIDEWALK RAMP DETAIL
NOT TO SCALE



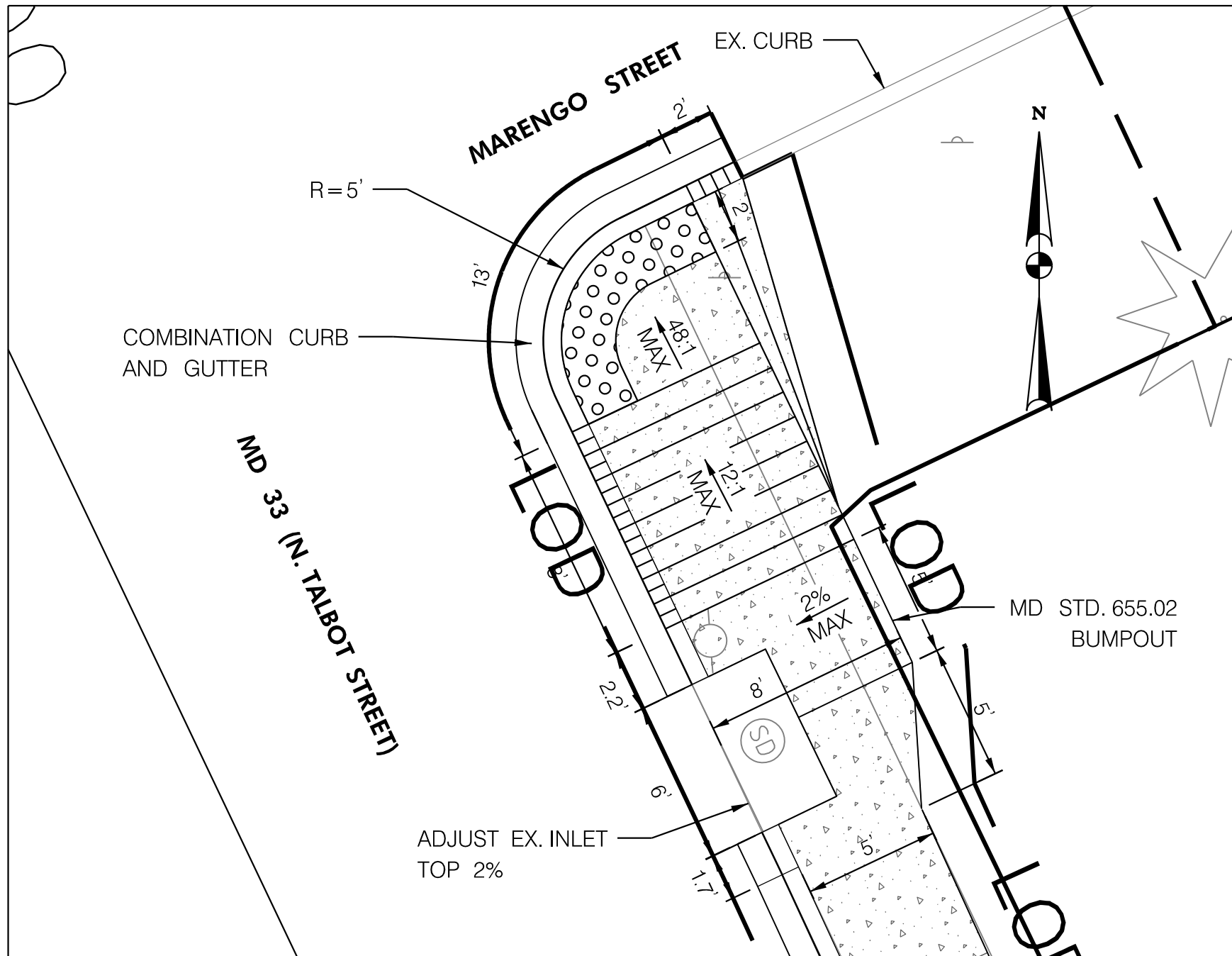
STA 57+32 RT. SIDEWALK RAMP DETAIL
NOT TO SCALE



STA 57+34 LT. SIDEWALK RAMP DETAIL
NOT TO SCALE



STA 59+85 RT. SIDEWALK RAMP DETAIL
NOT TO SCALE



STA 60+18 LT. SIDEWALK RAMP DETAIL
NOT TO SCALE

NOTE TO CONTRACTOR:

- ALL DISTURBED AREAS SHALL BE TEMPORARILY STABILIZED AT THE END OF EACH WORK DAY WITH TYPE 'E' SOIL STABILIZATION MATTING IN PERVIOUS AREAS OR GRADED AGGREGATE BASE COURSE IN IMPERVIOUS AREAS.
- NO DISTURBED AREA SHALL BE LEFT UNSTABILIZED OVERNIGHT UNLESS THE RUNOFF IS DIRECTED TO AN MDE APPROVED SEDIMENT CONTROL DEVICE.
- SEE GENERAL NOTES ON SHEET DE-01 FOR ADDITIONAL CONSTRUCTION NOTES FOR CURB RAMPS, SIDEWALKS, AND MEDIANS.
- ALL TRANSITION SIDEWALK PANELS MUST BE CONSTRUCTED 5' MINIMUM OR TO THE NEXT EXISTING JOINT OR AS DIRECTED BY THE ENGINEER.

PAVEMENT LEGEND			
	BRICK SIDEWALK		CONCRETE SIDEWALK
	STAMPED CONCRETE		CONCRETE DRIVEWAY ENTRANCE
	SIDEWALK REMOVAL		DRIVEWAY TIE-IN
	SIDEWALK RAMP	- LOD - LIMIT OF DISTURBANCE	
	DETECTABLE WARNING SURFACE	- CBCA - CHESAPEAKE BAY CRITICAL AREA	

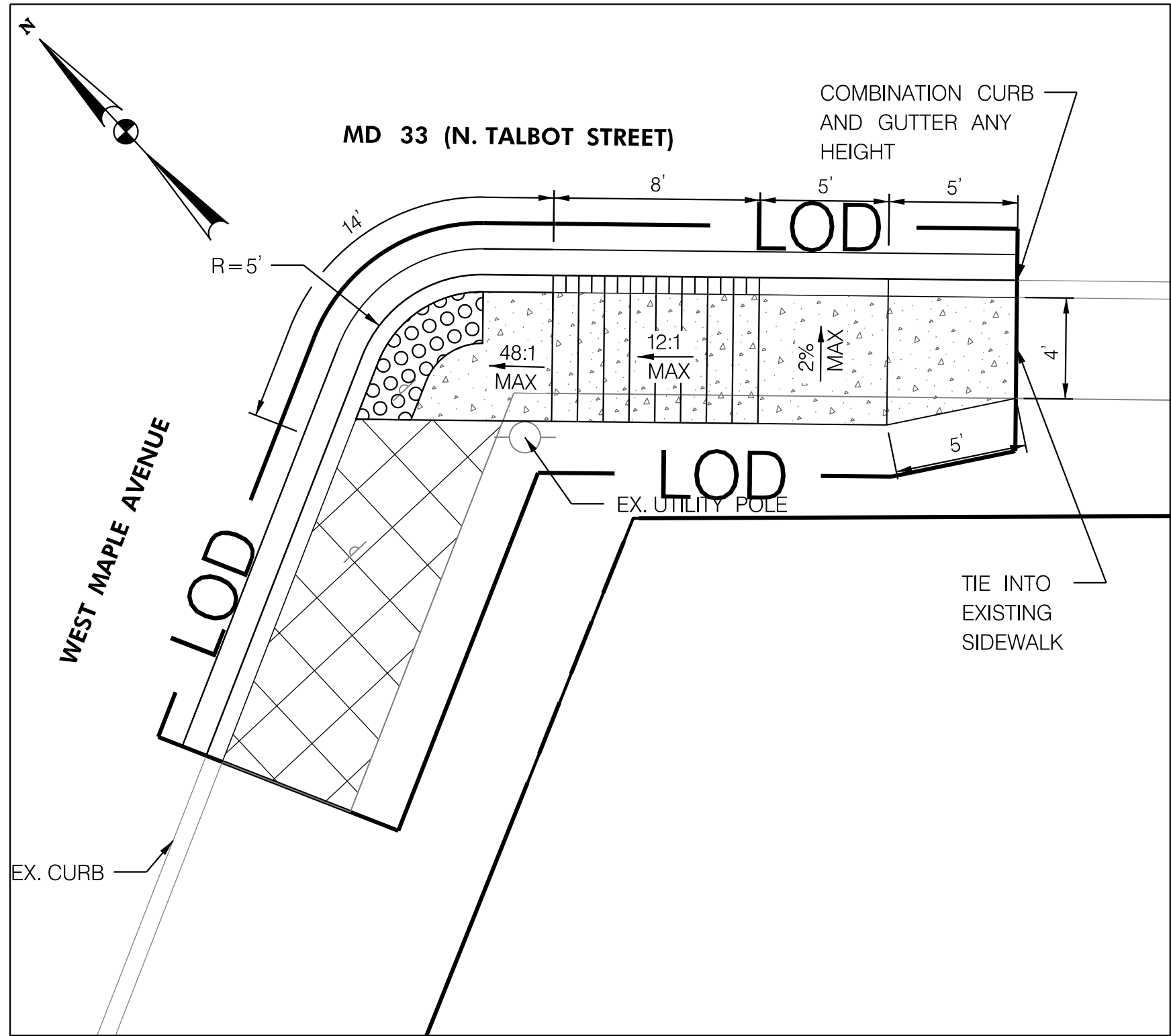
R / W PLAT NUMBER	REVISIONS	SIDEWALK DETAIL SHEET	
		SCALE AS SHOWN	DATE MARCH 2020 CONTRACT NO. TA2295133
		DESIGNED BY AUO	COUNTY TALBOT COUNTY
		DRAWN BY PNK	LOGMILE 12.852 TO 13.983
		CHECKED BY RJM	HORIZONTAL SCALE N/A
		MDE/PRD 17-PR-0056-04	VERTICAL SCALE N/A
		DRAWING NO. SD-06 OF SD-07	SHEET NO. 10 OF 26



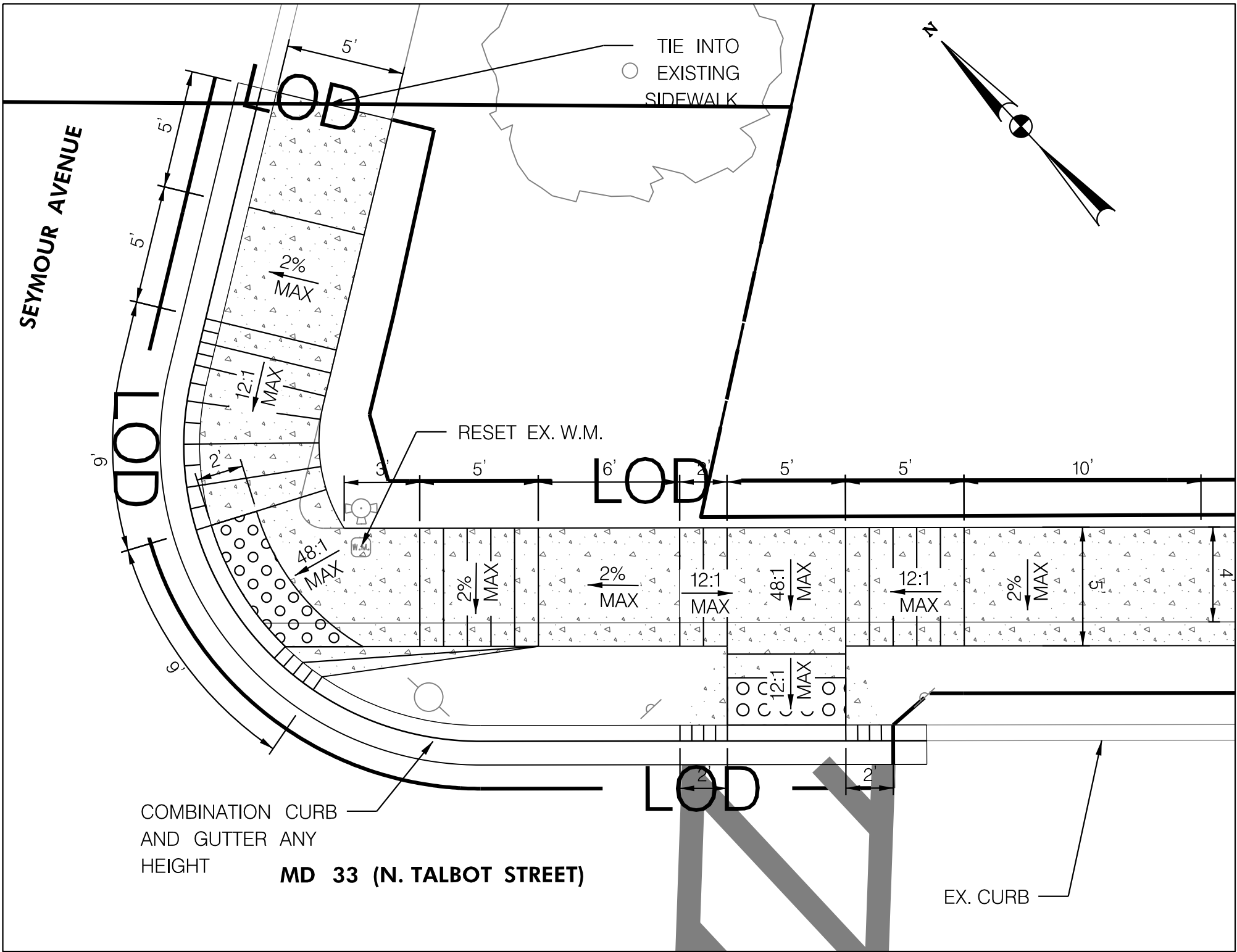
INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

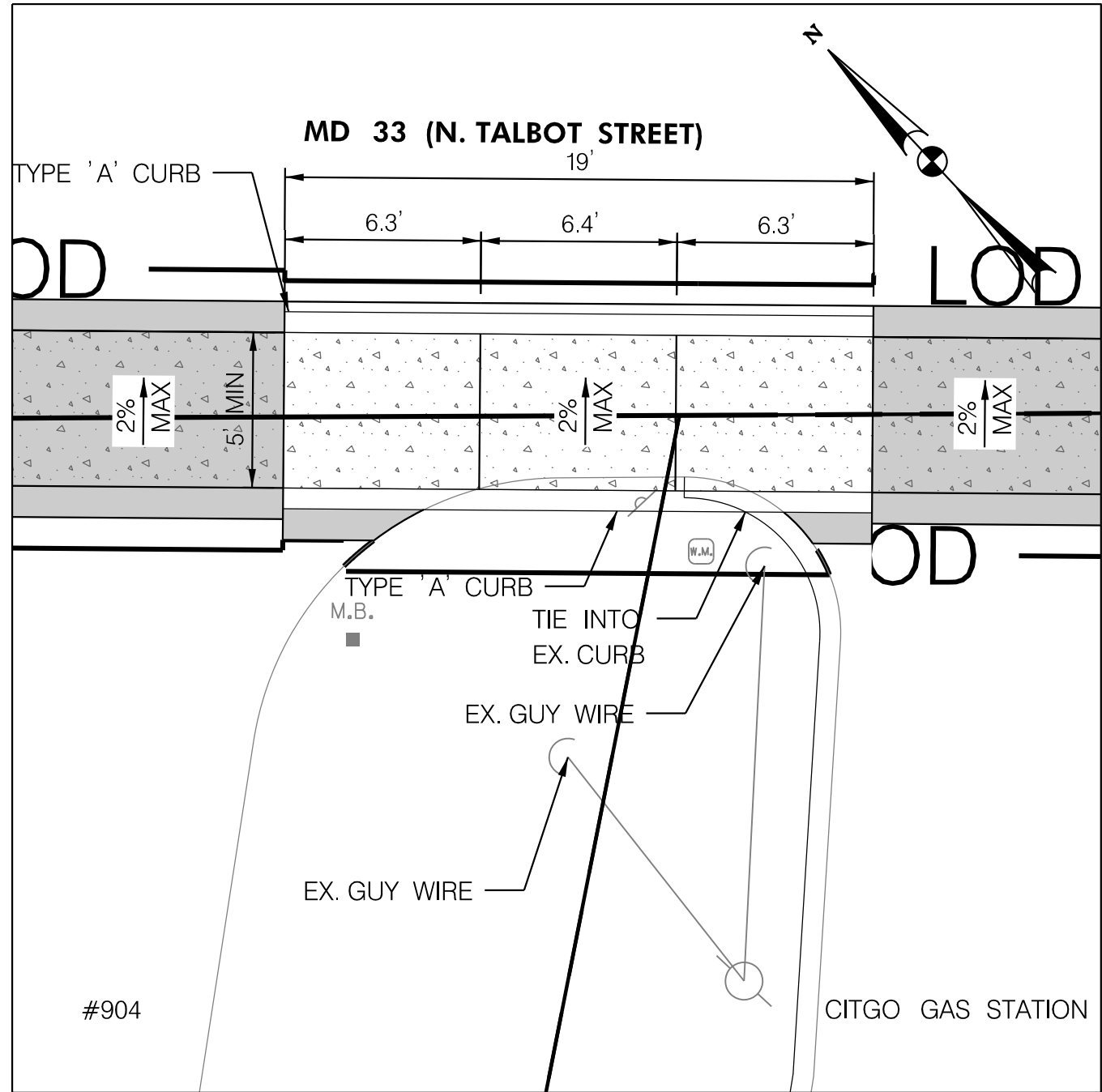
MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE



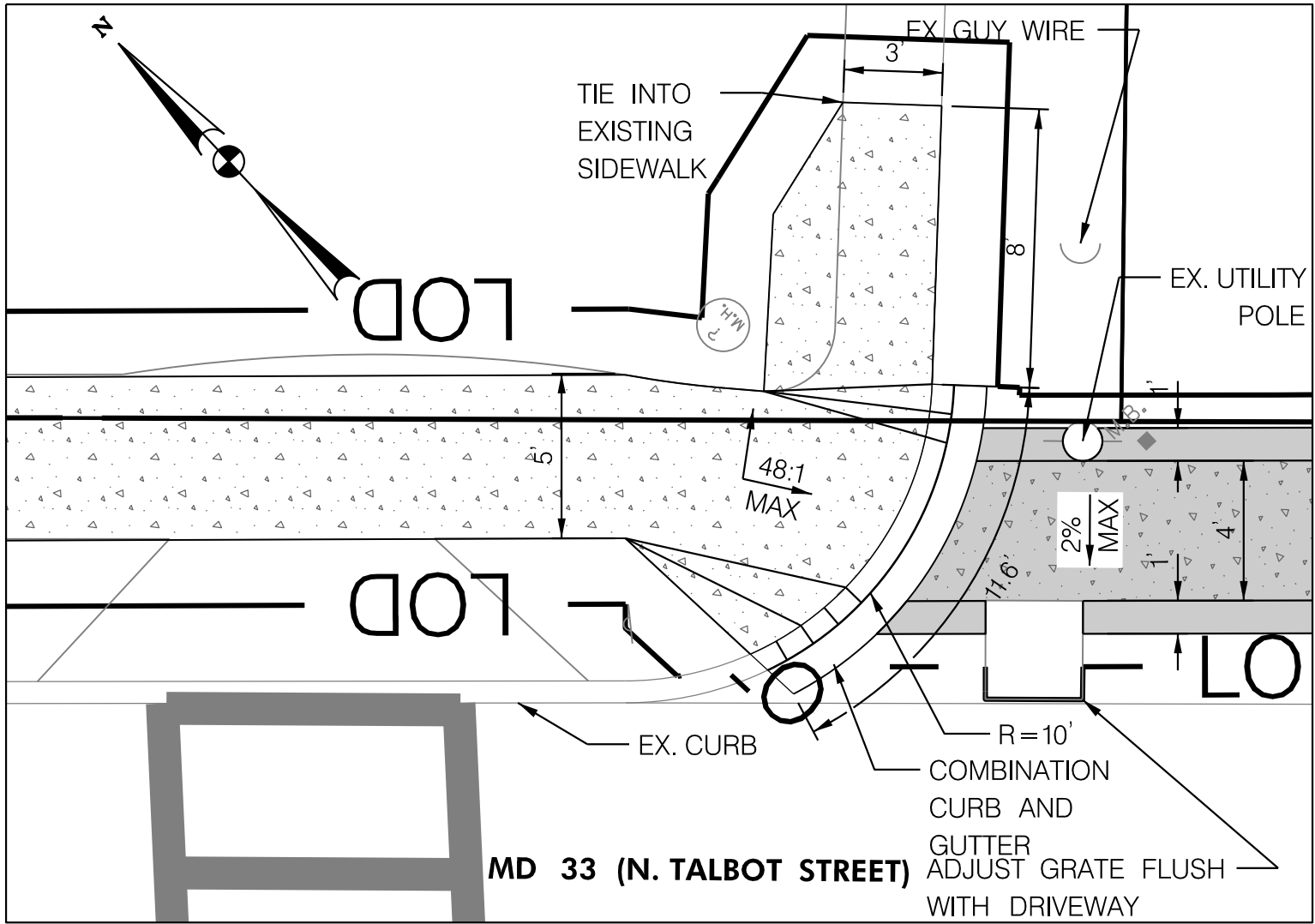
STA 63+19 RT. SIDEWALK RAMP DETAIL
(ADA WAIVER LOCATION)
NOT TO SCALE



STA 66+97 LT. SIDEWALK RAMP DETAIL
NOT TO SCALE



STA 68+74 RT. SIDEWALK CUT THROUGH DETAIL
NOT TO SCALE

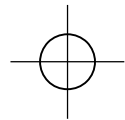
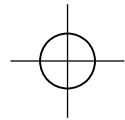
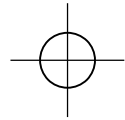


STA 72+45 LT. SIDEWALK RAMP DETAIL
NOT TO SCALE

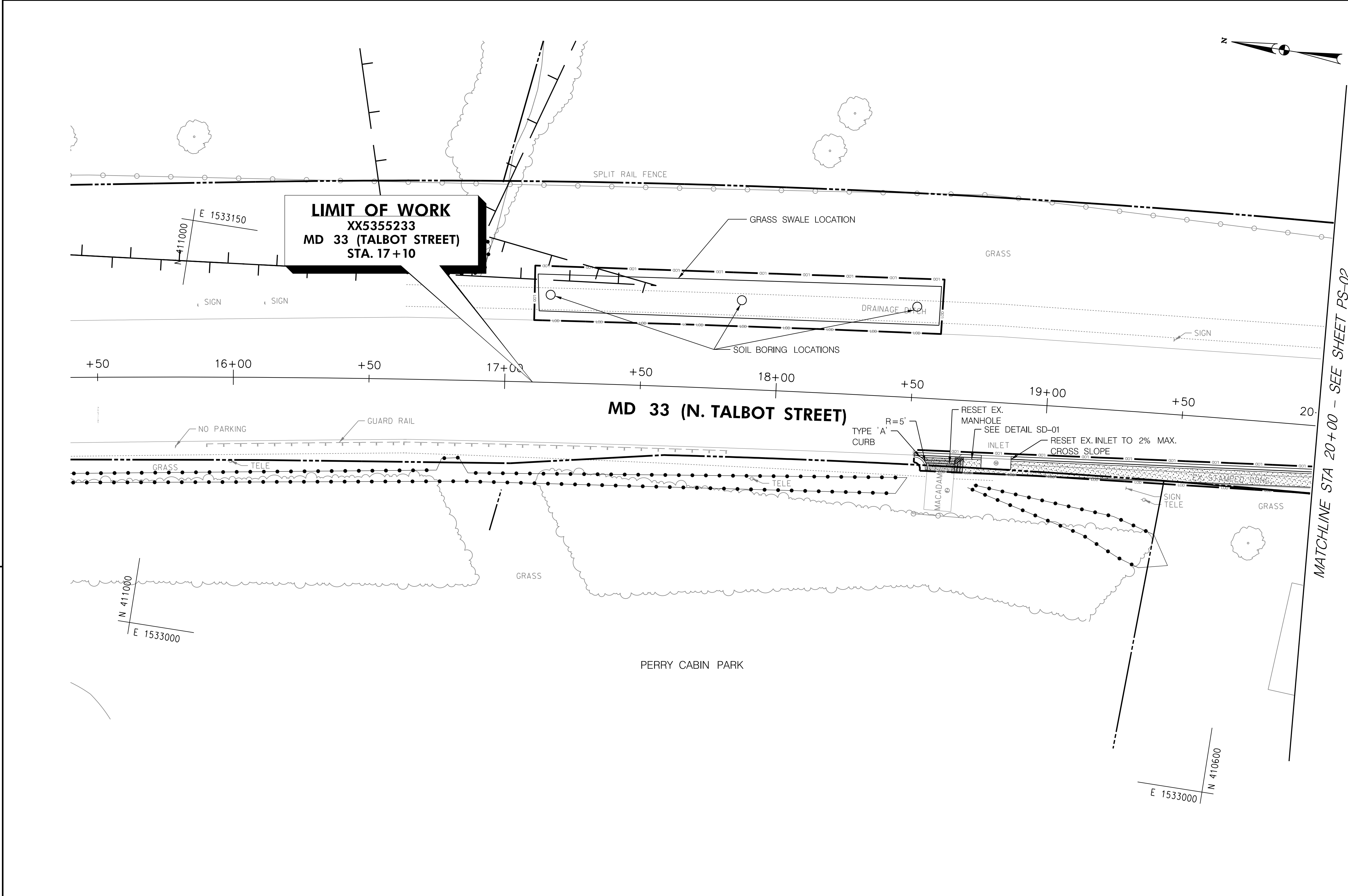
NOTE TO CONTRACTOR:

- ALL DISTURBED AREAS SHALL BE TEMPORARILY STABILIZED AT THE END OF EACH WORK DAY WITH TYPE 'E' SOIL STABILIZATION MATTING IN PERVIOUS AREAS OR GRADED AGGREGATE BASE COURSE IN IMPERVIOUS AREAS.
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- ALL TRANSITION SIDEWALK PANELS MUST BE CONSTRUCTED 5' MINIMUM OR TO THE NEXT EXISTING JOINT OR AS DIRECTED BY THE ENGINEER.

PAVEMENT LEGEND		R / W PLAT NUMBER	REVISIONS	SIDEWALK DETAIL SHEET	
	BRICK SIDEWALK		CONCRETE SIDEWALK	SCALE AS SHOWN	DATE: MARCH 2020 CONTRACT NO. TA2295133
	STAMPED CONCRETE		CONCRETE DRIVEWAY ENTRANCE	DESIGNED BY: AUO	COUNTY: TALBOT COUNTY
	SIDEWALK REMOVAL		DRIVEWAY TIE-IN	DRAWN BY: PNK	LOGMILE: 12.852 TO 13.983
	SIDEWALK RAMP	- LOD - LIMIT OF DISTURBANCE		CHECKED BY: RJM	HORIZONTAL SCALE: N/A
	DETECTABLE WARNING SURFACE	- CBCA - CHESAPEAKE BAY CRITICAL AREA		MDE/PRD: 17-PR-0056-04	VERTICAL SCALE: N/A
				DRAWING NO. SD-07	OF SD-07 SHEET NO. 11 OF 26



BY: eyanb -



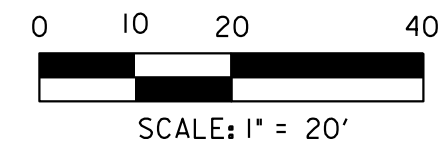
THIS SHEET IS LOCATED ENTIRELY WITHIN
THE CHESAPEAKE BAY CRITICAL AREA

NOTE TO CONTRACTOR:

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- DRIVEWAY TIE-INS SHOULD MATCH EXISTING MATERIAL (ASPHALT/GRAVEL/BRICK)

ROADWAY LEGEND

●—● WETLAND	BRICK SIDEWALK	CONCRETE SIDEWALK
— 100-YEAR FLOOD PLAIN	SIDEWALK REMOVAL	CONCRETE DRIVEWAY ENTRANCE
— CBCA — CHESAPEAKE BAY CRITICAL AREA	SIDEWALK RAMP	DRIVEWAY TIE-IN (SEE NOTE 5)
— WUS — WATERS OF THE US	DETECTABLE WARNING SURFACE	— LOD — LIMIT OF DISTURBANCE



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

QUANTITY NOTES

8 INCH PORTLAND CEMENT CONCRETE
PAVEMENT FOR DRIVEWAY MIX 9

STATION	S.Y.	REMARKS
STA. 18+55, RT TO STA. 18+70, RT	8	

TYPE A CURB ANY HEIGHT OR DEPTH

STATION	L.F.	REMARKS
STA. 18+52, 22' RT TO STA. 18+57, 26' RT	8	

TYPE A COMBINATION CURB AND GUTTER
ANY HEIGHT OR DEPTH

STATION	L.F.	REMARKS
STA. 18+52, 21' RT TO STA. 18+77, 21' RT	25	
STA. 18+88, 21' RT TO STA. 20+00, 24' RT	112	

5 INCH CONCRETE SIDEWALK

STATION	S.F.	REMARKS
STA. 18+70, RT TO STA. 20+00, RT	615	

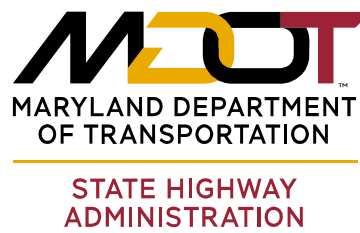
TURFGRASS SOD ESTABLISHMENT

STATION	S.Y.	REMARKS
STA. 18+50, RT TO STA. 18+57, RT	3	
STA. 18+67, RT TO STA. 20+00, RT	32	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

STATION	S.Y.	REMARKS
STA. 18+50, RT TO STA. 18+57, RT	3	
STA. 18+67, RT TO STA. 20+00, RT	32	

NOTE: SAME-DAY STABILIZATION SHALL BE PROVIDED FOR ALL
DISTURBED AREAS.



INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

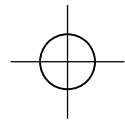
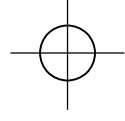
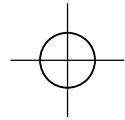
MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

ROADWAY PLAN SHEET

SCALE 1" = 20' DATE MARCH 2020 CONTRACT NO. TA2295133

DESIGNED BY AUJ COUNTY TALBOT COUNTY
DRAWN BY PNK LOGMILE 12.852 TO 13.983
CHECKED BY RJM HORIZONTAL SCALE N/A
MDE/PRD 17-PR-0056-04 VERTICAL SCALE N/A

DRAWING NO. PS-01 OF PS-14 SHEET NO. 12 OF 26

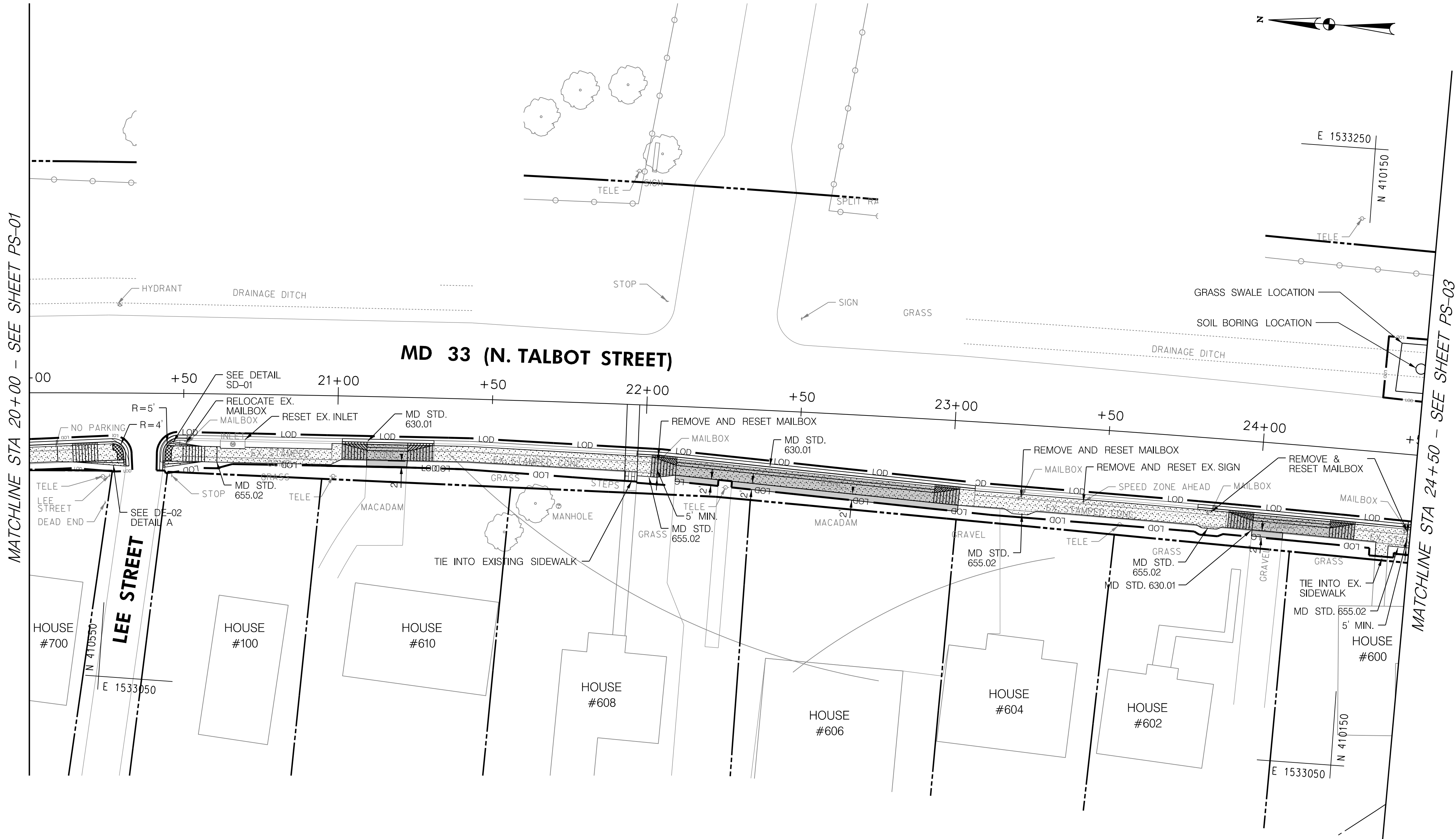


BY: eyanb -

THIS SHEET IS LOCATED ENTIRELY WITHIN
THE CHESAPEAKE BAY CRITICAL AREA
NOTE TO CONTRACTOR:

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- DRIVEWAY TIE-INS SHOULD MATCH EXISTING MATERIAL (ASPHALT/GRAVEL/BRICK)

PLOTTED: 12/15/2020
FILE: pw:\ghawm\pw\shacadd\ad\mdot\mdstate\SHAEDMS01\Documents\A\areawide Projects\AW-ADA Sidewalks Design\Fund 33 Projects\District 2\MTA\Design Submittal No. 8 - MD 33 from Lee St to Spencer Dr\SHA In-house\Roadway\CAD\pHD-P002_MD 33.dgn



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

ROADWAY LEGEND

●—● WETLAND	BRICK SIDEWALK	CONCRETE SIDEWALK
— 100-YEAR FLOOD PLAIN	SIDEWALK REMOVAL	CONCRETE DRIVEWAY ENTRANCE
- CBCA - CHESAPEAKE BAY CRITICAL AREA	SIDEWALK RAMP	DRIVEWAY TIE-IN (SEE NOTE 5)
— WUS — WATERS OF THE US	DETECTABLE WARNING SURFACE	— LOD — LIMIT OF DISTURBANCE

R / W PLAT NUMBER

REVISIONS

QUANTITY NOTES

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9

STATION	S.Y.	REMARKS
STA. 21+02, RT TO STA. 21+31, RT	17	
STA. 22+02, RT TO STA. 23+03, RT	59	
STA. 23+91, RT TO STA. 24+32, RT	23	

5 INCH CONCRETE SIDEWALK

STATION	S.F.	REMARKS
STA. 20+01, RT TO STA. 20+31, RT	165	
STA. 20+44, RT TO STA. 21+02, RT	295	
STA. 21+31, RT TO STA. 22+02, RT	375	
STA. 23+03, RT TO STA. 23+91, RT	457	
STA. 24+32, RT TO STA. 24+50, RT	110	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH

STATION	L.F.	REMARKS
STA. 20+01, 17' RT TO STA. 20+31, 24' RT	37	
STA. 20+43, 25' RT TO STA. 20+51, 14' RT	16	
STA. 21+02, 15' RT TO STA. 21+37, 15' RT	30	
STA. 21+97, 16' RT TO STA. 23+08, 22' RT	110	
STA. 23+80, 23' RT TO STA. 24+50, 24' RT	70	

DETECTABLE WARNING SURFACE FOR CURB RAMPS

STATION	S.F.	REMARKS
STA. 20+27, 16' RT	12	
STA. 20+44, 22' RT	10	

TURFGRASS SOD ESTABLISHMENT

STATION	S.Y.	REMARKS
STA. 20+00, RT TO STA. 20+32, RT	8	
STA. 20+44, RT TO STA. 21+10, RT	15	
STA. 21+23, RT TO STA. 21+94, RT	16	
STA. 21+98, RT TO STA. 22+10, RT	2	
STA. 22+24, RT TO STA. 22+29, RT	1	
STA. 23+16, RT TO STA. 23+98, RT	19	
STA. 24+09, RT TO STA. 24+39, RT	8	
STA. 24+43, RT TO STA. 24+50, RT	3	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

STATION	S.Y.	REMARKS
STA. 20+00, RT TO STA. 20+32, RT	8	
STA. 20+44, RT TO STA. 21+10, RT	15	
STA. 21+23, RT TO STA. 21+94, RT	16	
STA. 21+98, RT TO STA. 22+10, RT	2	
STA. 22+24, RT TO STA. 22+29, RT	1	
STA. 23+16, RT TO STA. 23+98, RT	19	
STA. 24+09, RT TO STA. 24+39, RT	8	
STA. 24+43, RT TO STA. 24+50, RT	3	

BRICK SIDEWALK

STATION	S.F.	REMARKS
STA. 21+94, RT TO STA. 21+98, RT	13	

NOTE: SAME-DAY STABILIZATION SHALL BE PROVIDED FOR ALL DISTURBED AREAS.



INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

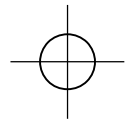
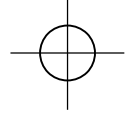
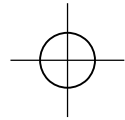
MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

ROADWAY PLAN SHEET

SCALE 1" = 20' DATE: MARCH 2020 CONTRACT NO. TA2295133

DESIGNED BY AUO COUNTY TALBOT COUNTY
DRAWN BY PNK LOGMILE 12.852 TO 13.983
CHECKED BY RJM HORIZONTAL SCALE N/A
MDE/PRD 17-PR-0056-04 VERTICAL SCALE N/A

DRAWING NO. PS-02 OF PS-14 SHEET NO. 13 OF 26



BY: eyanb -

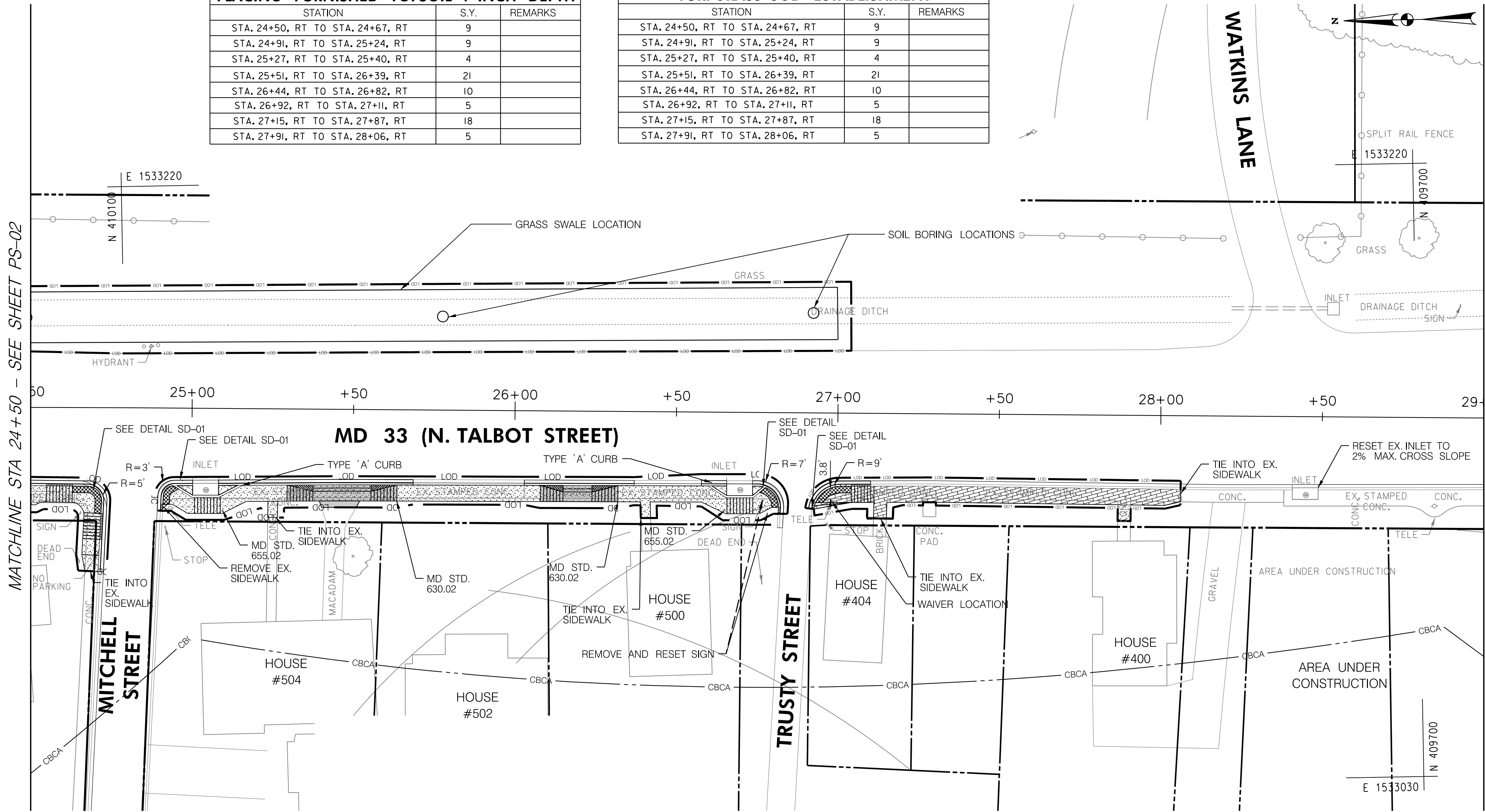
THIS SHEET IS LOCATED ENTIRELY WITHIN
THE CHESAPEAKE BAY CRITICAL AREA
NOTE TO CONTRACTOR:

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- DRIVEWAY TIE-INS SHOULD MATCH EXISTING MATERIAL (ASPHALT/GRAVEL/BRICK)

PLOTTED: 12/15/2020
FILE: p:\c\ghem\pww\shacadd\ad\mdot\mdstate\SHAEDMS01\Documents\A\reawide Projects\AW-ADA Sidewalks Design\Fund 33 Projects\District 2\MTA\Design Submittal No. 8 - MD 33 from Lee St to Spencer Dr\SHA In-house\Roadway\CAD\pH-D-P003_MD 33.dgn

PLACING FURNISHED TOPSOIL 4 INCH DEPTH		
STATION	S.Y.	REMARKS
STA. 24+50, RT TO STA. 24+67, RT	9	
STA. 24+91, RT TO STA. 25+24, RT	9	
STA. 25+27, RT TO STA. 25+40, RT	4	
STA. 25+51, RT TO STA. 26+39, RT	21	
STA. 26+44, RT TO STA. 26+82, RT	10	
STA. 26+92, RT TO STA. 27+11, RT	5	
STA. 27+15, RT TO STA. 27+87, RT	18	
STA. 27+91, RT TO STA. 28+06, RT	5	

TURFGRASS SOD ESTABLISHMENT		
STATION	S.Y.	REMARKS
STA. 24+50, RT TO STA. 24+67, RT	9	
STA. 24+91, RT TO STA. 25+24, RT	9	
STA. 25+27, RT TO STA. 25+40, RT	4	
STA. 25+51, RT TO STA. 26+39, RT	21	
STA. 26+44, RT TO STA. 26+82, RT	10	
STA. 26+92, RT TO STA. 27+11, RT	5	
STA. 27+15, RT TO STA. 27+87, RT	18	
STA. 27+91, RT TO STA. 28+06, RT	5	



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

ROADWAY LEGEND		
WETLAND	BRICK SIDEWALK	CONCRETE SIDEWALK
100-YEAR FLOOD PLAIN	SIDEWALK REMOVAL	CONCRETE DRIVEWAY ENTRANCE
CHESAPEAKE BAY CRITICAL AREA	SIDEWALK RAMP	DRIVEWAY TIE-IN (SEE NOTE 5)
WUS - WATERS OF THE US	DETECTABLE WARNING SURFACE	LOD - LIMIT OF DISTURBANCE

R / W PLAT NUMBER	REVISIONS
ROADWAY PLAN SHEET	
SCALE: 1" = 20' DATE: MARCH 2020 CONTRACT NO. TA2295133	
DESIGNED BY: AUO	COUNTY: TALBOT COUNTY
DRAWN BY: PNK	LOGMILE: 12.852 TO 13.983
CHECKED BY: RJM	HORIZONTAL SCALE: N/A
MDE/PRD: 17-PR-0056-04	VERTICAL SCALE: N/A
DRAWING NO. PS-03 OF PS-14	SHEET NO. 14 OF 26

QUANTITY NOTES

5 INCH CONCRETE SIDEWALK		
STATION	S.F.	REMARKS
STA. 24+50, RT TO STA. 24+72, RT	200	
STA. 24+93, RT TO STA. 25+30, RT	216	
STA. 24+96, RT TO STA. 25+00, RT	7	
STA. 25+08, RT TO STA. 25+14, RT	9	
STA. 25+24, RT TO STA. 25+27, RT	15	
STA. 25+64, RT TO STA. 26+08, RT	222	
STA. 26+32, RT TO STA. 26+80, RT	270	
STA. 26+61, RT TO STA. 26+66, RT	7	
STA. 26+74, RT TO STA. 26+77, RT	5	
STA. 27+87, RT TO STA. 27+92, RT	20	

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9		
STATION	S.Y.	REMARKS
STA. 25+30, RT TO STA. 25+64, RT	19	
STA. 26+08, RT TO STA. 26+32, RT	14	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH		
STATION	L.F.	REMARKS
STA. 24+50, 24' RT TO STA. 24+72, 24' RT	48	
STA. 24+90, 32' RT TO STA. 25+00, 23' RT	16	
STA. 25+08, 23' RT TO STA. 25+69, 23' RT	60	
STA. 26+03, 22' RT TO STA. 26+37, 22' RT	34	
STA. 26+58, 22' RT TO STA. 26+66, 22' RT	8	
STA. 26+74, 22' RT TO STA. 26+82, 31' RT	15	
STA. 26+92, 30' RT TO STA. 27+10, 22' RT	22	

TYPE A CURB ANY HEIGHT OR DEPTH		
STATION	L.F.	REMARKS
STA. 24+94, 24' RT TO STA. 25+16, 24' RT	23	
STA. 26+60, 23' RT TO STA. 26+78, 23' RT	21	

BRICK SIDEWALK		
STATION	S.F.	REMARKS
STA. 26+93, RT TO STA. 28+06, RT	580	

DETECTABLE WARNIGN SURFACE FOR CURB RAMPS		
STATION	S.F.	REMARKS
STA. 24+70, 27' RT	28	GRAY OR TAN
STA. 24+92, 27' RT	13	GRAY OR TAN
STA. 26+80, 26' RT	15	GRAY OR TAN
STA. 26+98, 24' RT	23	GRAY OR TAN

CLASS 2 EXCAVATION		
STATION	C.Y.	REMARKS
STA. 24+91, RT TO STA. 24+93, RT	1	

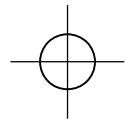
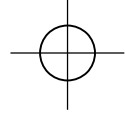
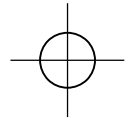
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INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

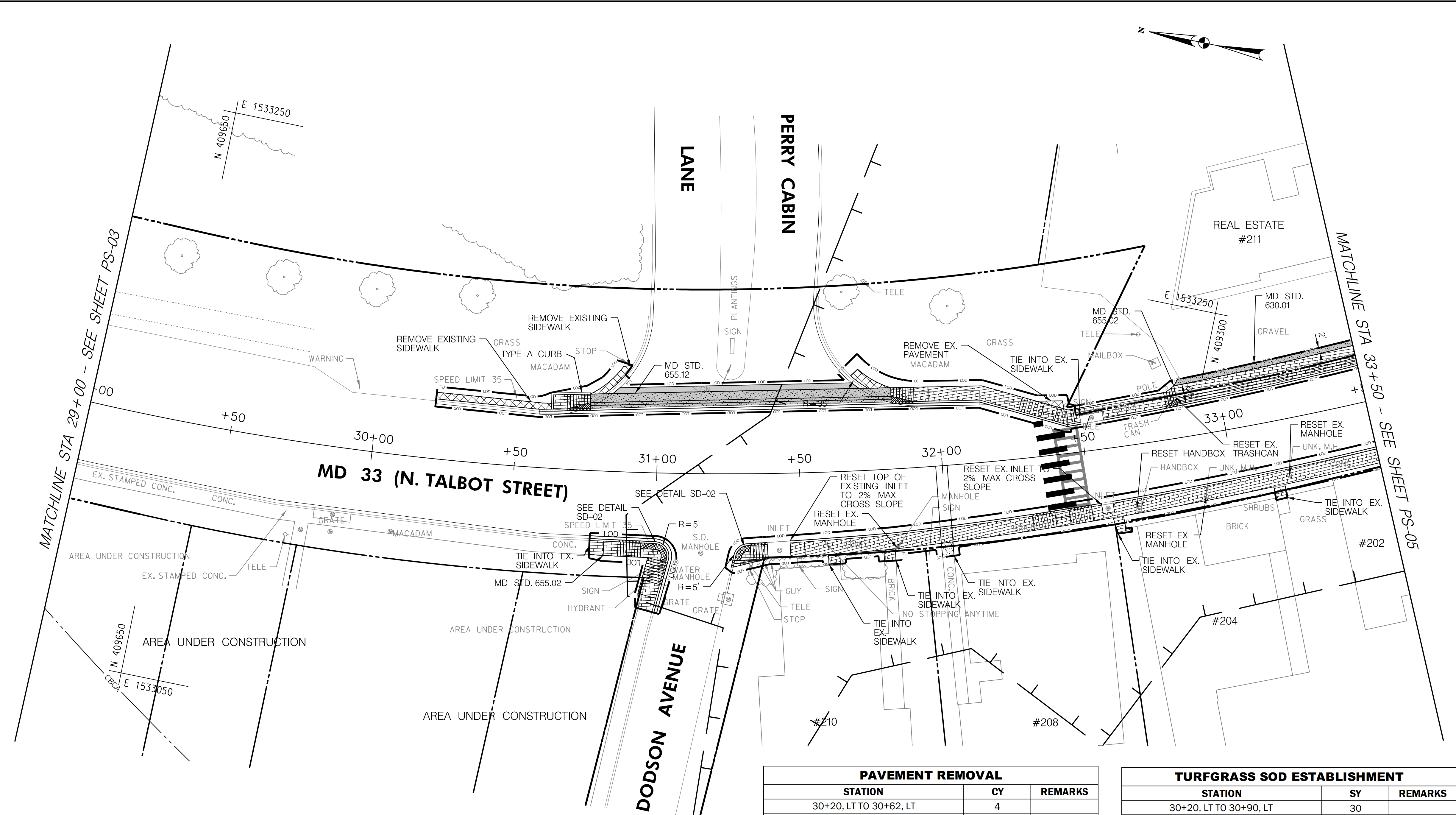


BY: eyanb -

THIS SHEET IS LOCATED ENTIRELY WITHIN
THE CHESAPEAKE BAY CRITICAL AREA
NOTE TO CONTRACTOR:

- ALL DISTURBED AREAS SHALL BE TEMPORARILY STABILIZED AT THE END OF EACH WORK DAY WITH TYPE 'E' SOIL STABILIZATION MATTING IN PERVIOUS AREAS OR GRADED AGGREGATE BASE COURSE IN IMPERVIOUS AREAS.
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- SEE GENERAL NOTES ON SHEET DE-01 FOR ADDITIONAL CONSTRUCTION NOTES FOR CURB RAMPS, SIDEWALKS, AND MEDIANS.
- ALL TRANSITION SIDEWALK PANELS MUST BE CONSTRUCTED 5' MINIMUM OR TO THE NEXT EXISTING JOINT OR AS DIRECTED BY THE ENGINEER.
- DRIVEWAY TIE-INS SHOULD MATCH EXISTING MATERIAL (ASPHALT/GRAVEL/BRICK)

PLOTTED: 12/15/2020
FILE: p:\chawm\pww\shacadd\ad\mdot\mdstate\SHAEDMS01\Documents\Areawide Projects\AW-ADA Sidewalks Design\Fund 33 Projects\District 2\MTA\Design Submittal No. 8 - MD 33 from Lee St to Spencer Dr\SHA In-house\Roadway\CAD\pHD-P004_MD 33.dgn



PAVEMENT REMOVAL		
STATION	CY	REMARKS
30+20, LT TO 30+62, LT	4	
30+75, LT TO 30+90, LT	2	
31+69, LT TO 31+84, LT	2	
32+35, LT TO 32+45, LT	1	

TYPE A CURB ANY HEIGHT OR DEPTH		
STATION	LF	REMARKS
30+62, 21' LT TO 30+75, 27' LT	20	

TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH		
STATION	L.F.	REMARKS
30+78, 24' RT TO 31+05, 30' RT	52	

TURFGRASS SOD ESTABLISHMENT		
STATION	SY	REMARKS
30+20, LT TO 30+90, LT	30	
30+78, RT TO 30+95, RT	8	
31+26, RT TO 31+59, RT	8	
31+63, RT TO 31+78, RT	4	
31+67, LT TO 32+86, LT	33	
31+82, RT TO 31+98, RT	5	
32+02, RT TO 32+57, RT	13	
32+60, RT TO 33+13, RT	13	
33+17, RT TO 33+50, RT	8	



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

QUANTITY NOTES

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9

STATION	S.Y	REMARKS
30+75, LT TO 31+83, LT	59	
32+85, LT TO 33+50, LT	15	2FT TIE IN

CLASS 2 EXCAVATION

STATION	C.Y.	REMARKS
30+20, LT TO 30+62, LT	4	
30+75, LT TO 30+90, LT	2	
31+69, LT TO 31+84, LT	2	
32+35, LT TO 32+45, LT	1	

GRANITE CURB

STATION	L.F.	REMARKS
30+58, 20' LT TO 32+45, 10' LT	186	
31+26, 33' RT TO 31+38, 24' RT	18	
31+46, 24' RT TO 32+54, 19' RT	110	
32+57, 19' RT TO 33+50, 16' RT	94	

ADJUST OR REPLACE GRANITE CURB

STATION	L.F.	REMARKS
32+45, 9' LT TO 32+50, 10' LT	5	
32+58, 11' LT TO 33+50, 13' LT	91	

BRICK SIDEWALK

STATION	S.F.	REMARKS
30+78, RT TO 31+04, RT	227	
30+62, LT TO 30+76, LT	65	
31+27, RT TO 31+38, RT	52	
31+46, RT TO 33+50, RT	1110	
31+83, LT TO 33+50, LT	910	

DETECTABLE WARNING SURFACE USING BRICK PAVERS

STATION	S.F.	REMARKS
31+00, 27' RT	24	
31+29, 27' RT	14	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

STATION	S.Y.	REMARKS
30+20, LT TO 30+90, LT	30	
30+78, RT TO 30+95, RT	8	
31+26, RT TO 31+59, RT	8	
31+63, RT TO 31+78, RT	4	
31+67, LT TO 32+86, LT	33	
31+82, RT TO 31+98, RT	5	
32+02, RT TO 32+57, RT	13	
32+60, RT TO 33+13, RT	13	
33+17, RT TO 33+50, RT	8	

5 INCH CONCRETE SIDEWALK

STATION	S.F.	REMARKS
31+98, 27' RT TO 32+02, 31' RT	19	

NOTE: SAME-DAY STABILIZATION SHALL BE PROVIDED FOR ALL DISTURBED AREAS.



INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

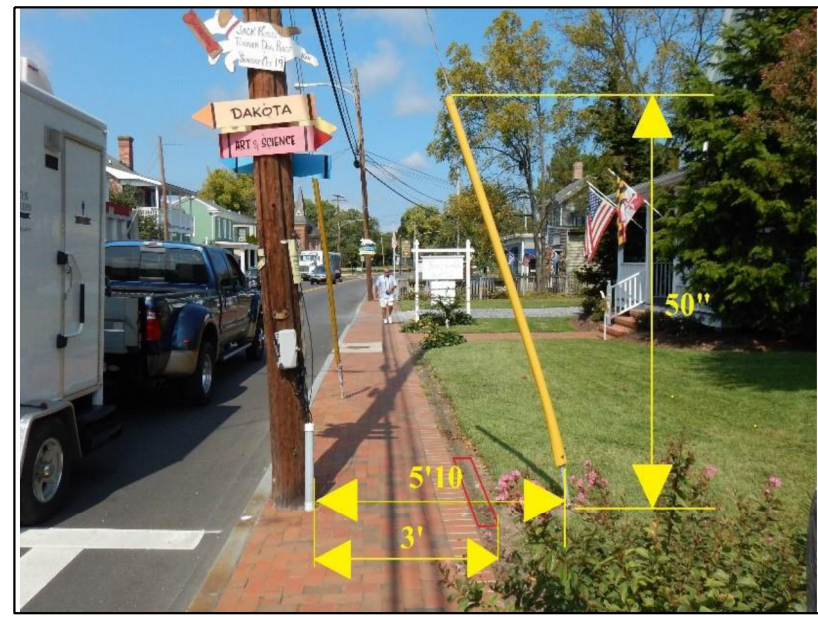
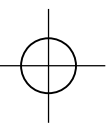
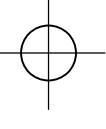
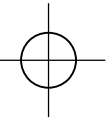
MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

ROADWAY PLAN SHEET

SCALE: 1" = 20' DATE: MARCH 2020 CONTRACT NO. TA2295133

DESIGNED BY: AUJ COUNTY: TALBOT COUNTY
DRAWN BY: PNK LOGMILE: 12.852 TO 13.983
CHECKED BY: RJM HORIZONTAL SCALE: N/A
MDE/PRD: 17-PR-0056-04 VERTICAL SCALE: N/A

DRAWING NO. PS-04 OF PS-14 SHEET NO. 15 OF 26

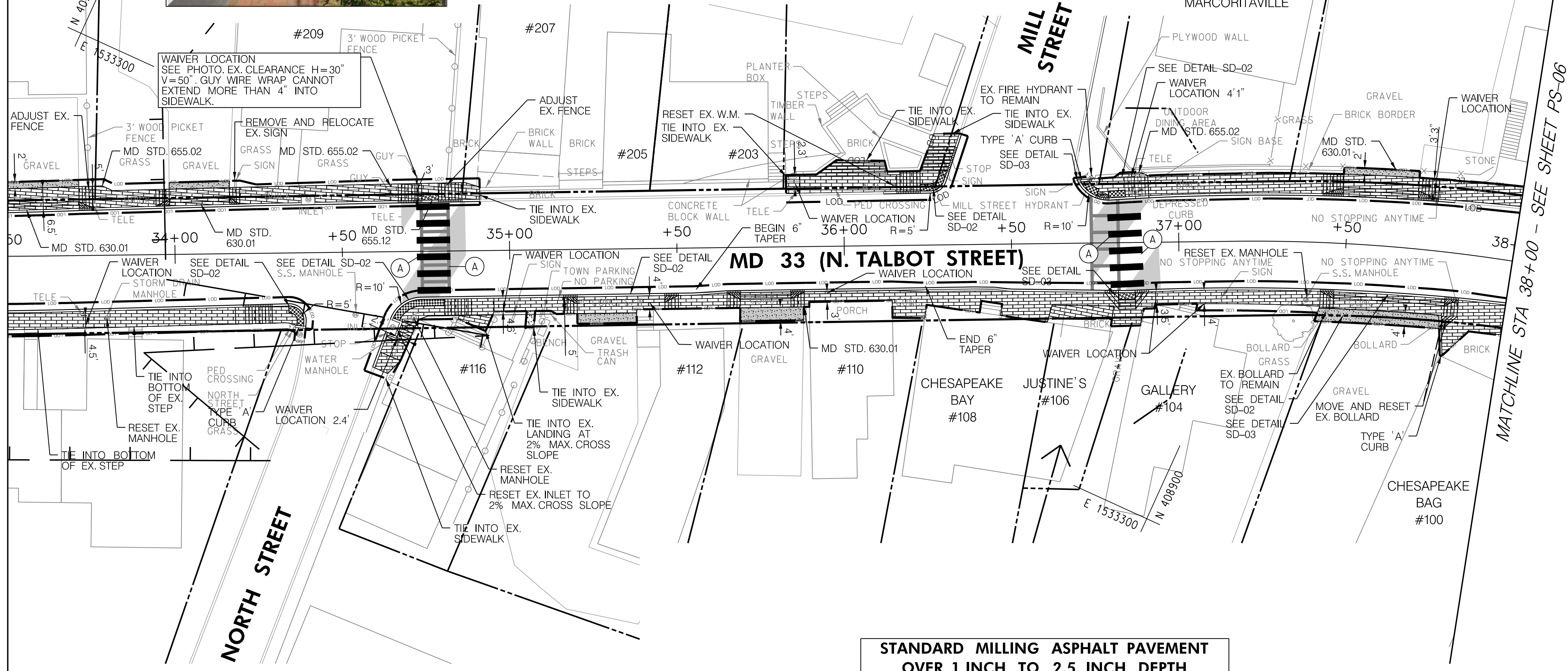


TURFGRASS SOD ESTABLISHMENT		
STATION	SY	REMARKS
33+50, RT TO 33+55, RT	1	
33+59, RT TO 33+86, RT	6	
33+74, LT TO 33+98, LT	6	
34+16, LT TO 34+31, LT	3	
34+38, LT TO 34+91, LT	12	
35+43, RT TO 35+69, RT	6	
37+05, RT TO 37+43, RT	8	

GRANITE CURB		
STATION	L.F	REMARKS
33+50, 16' RT TO 34+39, 22' RT	95	
34+59, 34' RT TO 34+62, 24' RT	11	
34+64, 20' RT TO 38+00, 12' RT	337	
36+11, 15' LT TO 36+36, 33' LT	35	

MATCHLINE STA 33+50 - SEE SHEET PS-04

MATCHLINE STA 38+00 - SEE SHEET PS-06



STANDARD MILLING ASPHALT PAVEMENT OVER 1 INCH TO 2.5 INCH DEPTH		
STATION	S.Y.	REMARKS
STA. 34+67 TO STA. 34+86	61	
STA. 36+73 TO STA. 36+95	61	

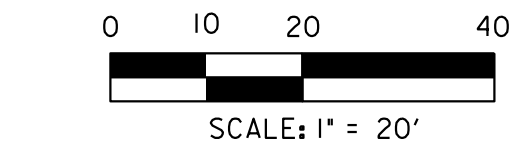
SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE PG 64E-22, LEVEL 2		
STATION	S.Y.	REMARKS
STA. 34+67 TO STA. 34+86	7.2	
STA. 36+73 TO STA. 36+95	7.2	

PAVEMENT MARKING LEGEND
(A) 24 INCH WHITE THERMOPLASTIC PAVEMENT MARKING (SOLID)

THIS SHEET IS LOCATED ENTIRELY WITHIN
THE CHESAPEAKE BAY CRITICAL AREA
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- DRIVEWAY TIE-INS SHOULD MATCH EXISTING MATERIAL (ASPHALT/GRAVEL/BRICK)

ROADWAY LEGEND		
●—● WETLAND	BRICK SIDEWALK	CONCRETE SIDEWALK
— 100-YEAR FLOOD PLAIN	SIDEWALK REMOVAL	CONCRETE DRIVEWAY ENTRANCE
= CBCA = CHESAPEAKE BAY CRITICAL AREA	SIDEWALK RAMP	DRIVEWAY TIE-IN (SEE NOTE 5)
— WUS — WATERS OF THE US	DETECTABLE WARNING SURFACE	LOD - LIMIT OF DISTURBANCE



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

QUANTITY NOTES

8 INCH PORTLAND CEMENT CONCRETE PAVEMENT FOR DRIVEWAY MIX 9

STATION	S.Y.	REMARKS
33+50, LT TO 33+71, LT	5	2FT TIE-IN
33+98, LT TO 34+17, LT	5	2FT TIE-IN
35+20, RT TO 35+38, RT	6	
35+69, RT TO 35+88, RT	8	
37+41, RT TO 37+81, RT	17	
37+48, LT TO 37+70, LT	5	2FT TIE-IN

TYPE A CURB ANY HEIGHT OR DEPTH

STATION	L.F	REMARKS
34+36, 22' RT TO 34+39, 23' RT	3	SEE DE-01
34+63, 25' RT TO 34+68, 21' RT	12	SEE DE-01
36+70, 19' LT TO 36+74, 19' LT	4	SEE DE-01
37+80, 19' RT TO 37+82, 31' RT	12	SEE DE-01

ADJUST OR REPLACE GRANITE CURB

STATION	L.F	REMARKS
33+50, 13' LT TO 34+91, 15' LT	142	**
36+70, 19' LT TO 37+81, 15' LT	116	

** MATCH EXISTING CURB HEIGHT AT TIE IN POINT

BRICK SIDEWALK

STATION	S.F.	REMARKS
33+50, LT TO 34+91, LT	728	
33+50, RT TO 34+39, RT	430	
34+59, RT TO 38+00, RT	1876	
35+83, LT TO 36+35, LT	405	
36+70, LT TO 38+00, LT	775	

DETECTABLE WARNING SURFACE FOR CURB RAMPS

STATION	S.F.	REMARKS
34+37, 19' RT	14	
34+72, 16' RT	33	
34+76, 16' LT	10	GRAY OR TAN
36+28, 18' LT	20	GRAY OR TAN
36+74, 17' LT	11	GRAY OR TAN
36+82, 14' LT	10	GRAY OR TAN
36+84, 16' RT	10	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH

STATION	S.Y.	REMARKS
33+50, RT TO 33+55, RT	1	
33+59, RT TO 33+86, RT	6	
33+74, LT TO 33+98, LT	6	
34+16, LT TO 34+31, LT	3	
34+38, LT TO 34+91, LT	12	
35+43, RT TO 35+69, RT	6	
37+05, RT TO 37+43, RT	8	

NOTE: SAME-DAY STABILIZATION SHALL BE PROVIDED FOR ALL DISTURBED AREAS.



INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

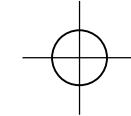
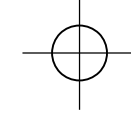
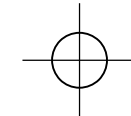
MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

ROADWAY PLAN SHEET

SCALE: 1" = 20' DATE: MARCH 2020 CONTRACT NO. TA2295133

DESIGNED BY: AUO COUNTY: TALBOT COUNTY
DRAWN BY: PNK LOGMILE: 12.852 TO 13.983
CHECKED BY: RJM HORIZONTAL SCALE: N/A
MDE/PRD: 17-PR-0056-04 VERTICAL SCALE: N/A

DRAWING NO. PS-05 OF PS-14 SHEET NO. 16 OF 26



BY: eyanb

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PLOTTED: 12/15/2020
FILE: p:\ch\h\m\p\w\sh\acadd\ad\mdot\mdstate\SHAEDMS01\Documents\Areawide Projects\AW-ADA Sidewalks Design\Fund 33 Projects\District 2\MTA\Design Submittal No. 8 - MD 33 from Lee St to Spencer Dr\SHA In-house\Roadway\CAD\pHD-P006_MD 33.dgn

EX. COLUMNS (5) TO REMAIN.
TIE INTO EX. COLUMNS
AS DIRECTED BY FIELD
ENGINEER

MATCHLINE STA 38+00 - SEE SHEET PS-05

RAILROAD AVENUE

CHERRY STREET

CARPENTER STREET

CARPENTER ALLEY

MATCHLINE STA 42+50 - SEE SHEET PS-07

SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE PG 64E-22, LEVEL 2			
STATION	TON	REMARKS	
STA. 38+18 TO STA. 38+39	7.5		
STA. 41+69 TO STA. 41+88	8		

STANDARD MILLING ASPHALT PAVEMENT OVER 1 INCH TO 2.5 INCH DEPTH			
STATION	S.Y.	REMARKS	
STA. 38+18 TO STA. 38+39	64		
STA. 41+69 TO STA. 41+88	68		

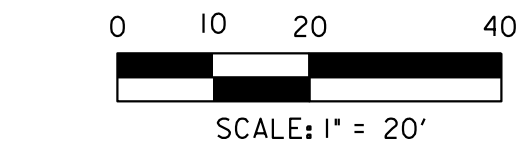
TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH		
STATION	LF	REMARKS
41+57, 40' LT TO 41+58, 35' LT	5	

GRANITE CURB		
STATION	L.F.	REMARKS
38+58, 60' RT TO 38+72, 12' RT	53	

ADJUST OR REPLACE GRANITE CURB		
STATION	L.F.	REMARKS
38+00, 13' RT TO 38+34, 43' RT	64	
38+17, 17' LT TO 38+50, 39' LT	53	..
38+70, 40' LT TO 38+88, 16' LT	25	..
39+90, 12' LT TO 40+36, 12' LT	47	
40+52, 21' LT TO 40+70, 33' LT	27	
40+86, 29' LT TO 41+04, 20' LT	24	..
41+38, 11' RT TO 41+59, 27' RT	32	
41+40, 20' LT TO 41+45, 20' LT	6	..
41+51, 20' LT TO 41+58, 35' LT	20	..
41+68, 27' RT TO 41+92, 13' RT	33	
41+73, 32' LT TO 41+93, 23' LT	27	..

.. MATCH EXISTING CURB HEIGHT AT TIE IN POINT

PAVEMENT MARKING LEGEND
(A) 24 INCH WHITE THERMOPLASTIC PAVEMENT MARKING (SOLID)



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

QUANTITY NOTES

TYPE A CURB ANY HEIGHT OR DEPTH		
STATION	LF	REMARKS
38+70, 37' RT TO 38+77, 26' RT	18	SEE DE-01
41+45, 21' LT TO 41+52, 24' LT	14	
41+53, 27' RT TO 41+58, 27' RT	6	SEE DE-01

BRICK SIDEWALK		
STATION	SF	REMARKS
38+00, RT TO 38+33, RT	485	
38+00, LT TO 38+50, LT	440	
38+59, RT TO 38+72, RT	295	
38+70, LT TO 38+88, LT	220	
39+85, RT TO 40+36, RT	845	
40+52, LT TO 40+69, LT	140	
40+87, LT TO 41+04, LT	130	
41+38, RT TO 41+58, RT	345	
41+40, LT TO 41+57, LT	150	
41+71, RT TO 41+92 RT	245	
41+74, LT TO 41+93, LT	120	

DETECTABLE WARNING SURFACE FOR CURB RAMPS		
STATION	SF	REMARKS
38+25, 14' RT	11	GRAY OR TAN
38+27, 20' LT	10	GRAY OR TAN
38+33, 32' RT	10	GRAY OR TAN
38+48, 22' LT	11	GRAY OR TAN
38+62, 36' RT	10	
38+71, 29' LT	15	GRAY OR TAN
41+55, 21' LT	17	GRAY OR TAN
41+56, 26' RT	11	GRAY OR TAN
41+69, 27' RT	17	GRAY OR TAN
41+74, 27' LT	9	GRAY OR TAN
41+76, 16' RT	11	GRAY OR TAN
41+80, 24' LT	10	GRAY OR TAN

NOTE: SAME-DAY STABILIZATION SHALL BE PROVIDED FOR ALL DISTURBED AREAS.



INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

ROADWAY PLAN SHEET

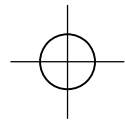
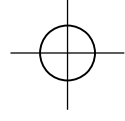
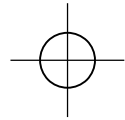
SCALE: 1" = 20' DATE: MARCH 2020 CONTRACT NO. TA2295133

DESIGNED BY AUO COUNTY TALBOT COUNTY
DRAWN BY PNK LOGMILE 12.852 TO 13.983
CHECKED BY RJM HORIZONTAL SCALE N/A
MDE/PRD 17-PR-0056-04 VERTICAL SCALE N/A

DRAWING NO. PS-06 OF PS-14 SHEET NO. 17 OF 26

ROADWAY LEGEND

WETLAND	BRICK SIDEWALK	CONCRETE SIDEWALK
100-YEAR FLOOD PLAIN	SIDEWALK REMOVAL	CONCRETE DRIVEWAY ENTRANCE
CHESAPEAKE BAY CRITICAL AREA	SIDEWALK RAMP	DRIVEWAY TIE-IN (SEE NOTE 5)
WATERS OF THE US	DETECTABLE WARNING SURFACE	LOD - LIMIT OF DISTURBANCE



BY: eyanb -

THIS SHEET IS LOCATED ENTIRELY WITHIN
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PAVEMENT MARKING LEGEND
(A) 24 INCH WHITE THERMOPLASTIC PAVEMENT MARKING (SOLID)

ROADWAY LEGEND

- | | | |
|---------------------------------------|----------------------------|------------------------------|
| ●—● WETLAND | BRICK SIDEWALK | CONCRETE SIDEWALK |
| — 100-YEAR FLOOD PLAIN | SIDEWALK REMOVAL | CONCRETE DRIVEWAY ENTRANCE |
| — CBCA — CHESAPEAKE BAY CRITICAL AREA | SIDEWALK RAMP | DRIVEWAY TIE-IN (SEE NOTE 5) |
| — WUS — WATERS OF THE US | DETECTABLE WARNING SURFACE | — LOD — LIMIT OF DISTURBANCE |

R / W PLAT NUMBER

REVISIONS



DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

QUANTITY NOTES

BRICK SIDEWALK		
STATION	S.F.	REMARKS
STA. 43+36, RT TO STA. 44+60, RT	1529	
STA. 43+64, LT TO STA. 44+32, LT	590	
STA. 44+67, LT TO STA. 44+80, LT	125	
STA. 46+32, LT TO STA. 47+00, LT	465	

DETECTABLE WARNING SURFACE FOR CURB RAMPS		
STATION	S.F.	REMARKS
STA. 43+80, 19' RT	10	
STA. 43+80, 20' LT	10	
STA. 44+23, 20' LT	19	
STA. 44+64, 19' LT	15	
STA. 45+91, 28' RT	15	
STA. 46+08, 26' RT	13	

ADJUST OR REPLACE GRANITE CURB		
STATION	L.F.	REMARKS
STA. 43+36, 16' RT TO STA. 43+88, 16' RT	52	
STA. 43+64, 20' LT TO STA. 44+32, 37' LT	82	
STA. 44+64, 24' LT TO STA. 44+78, 13' LT	23	
STA. 46+32, 14' LT TO STA. 47+00, 14' LT	66	

PLACING FURNISHED TOPSOIL 4 INCH DEPTH		
STATION	S.Y.	REMARKS
STA. 44+68, LT TO STA. 44+80, LT	1	
STA. 46+32, LT TO STA. 46+45, LT	3	

TURFGRASS SOD ESTABLISHMENT		
STATION	S.Y.	REMARKS
STA. 44+68, LT TO STA. 44+80, LT	1	
STA. 46+32, LT TO STA. 46+45, LT	3	

STANDARD MILLING ASPHALT PAVEMENT OVER 1 INCH TO 2.5 INCH DEPTH		
STATION	S.Y.	REMARKS
STA. 43+73 TO STA. 43+93	81	

SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE PG 64E-22, LEVEL 2		
STATION	TON	REMARKS
STA. 43+73 TO STA. 43+93	9.5	

NOTE: SAME-DAY STABILIZATION SHALL BE PROVIDED FOR ALL DISTURBED AREAS.



INNOVATIVE CONTRACTING DIVISION

REPLACEMENT AND REPAIR OF
SIDEWALKS IN DISTRICT 2 AREA WIDE

MD 33 (TALBOT STREET) 200' NORTH
OF LEE STREET TO
150' SOUTH OF SPENCER DRIVE

ROADWAY PLAN SHEET

SCALE: 1" = 20' DATE: MARCH 2020 CONTRACT NO. TA2295133

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MDE/PRD 17-PR-0056-04 VERTICAL SCALE N/A

DRAWING NO. PS-07 OF PS-14 SHEET NO. 18 OF 26